

**~STATE HIGHWAYS~
Archive Collection
Hampden County Registry of Deeds**



EAST LONGMEADOW



*Donald E. Ashe, Register
Hampden County Registry of Deeds,
a Division of the Office of
William Francis Galvin, Secretary of the Commonwealth*



1964	STATE HIGHWAY LAYOUT #5402 Route 83, North Main St. 2 sections, Westwood Ave. & Purves St. westerly
1964	STATE HIGHWAY LAYOUT #5443 Route 83, North Main St., bridge alteration over NYNH&H Railroad tracks
1986	STATE HIGHWAY LAYOUT #6742 Intersections: North Main St., Allen St., Parker St., Chestnut St., Shaker Rd.

1964

East Longmeadow



STATE HIGHWAY LAYOUT #5402
Route 83, North Main St. 2 sections, Westwood Ave. & Purves St. westerly

SEE HAMPDEN COUNTY REGISTRY OF DEEDS PLAN BOOK 96 PAGES 10-12.

INDEXING

STATE HIGHWAY LAYOUT #5402

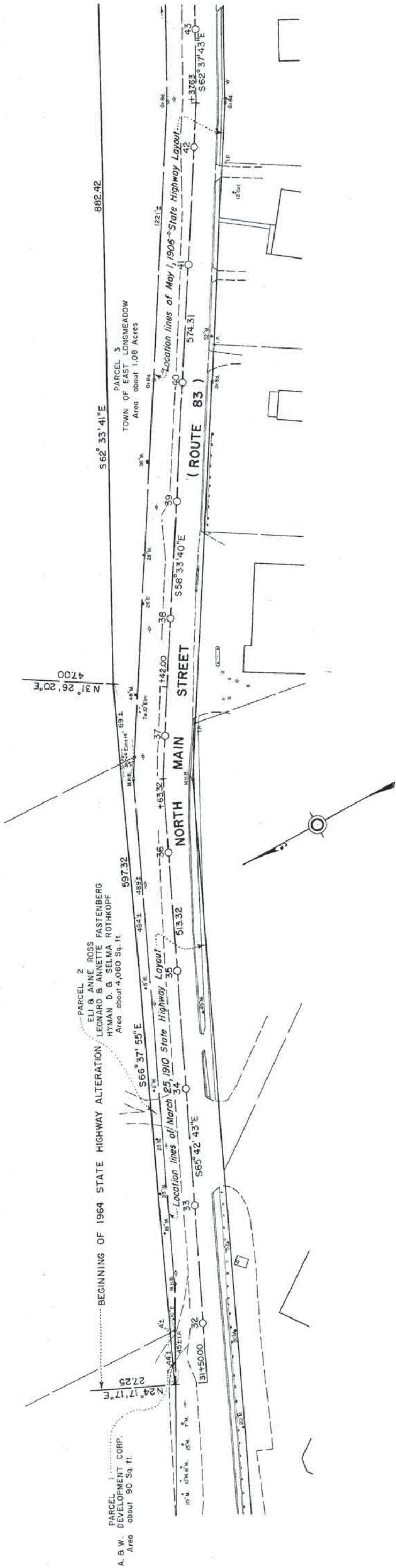
Image Info SH55001 East Longmeadow

Image Info SH55001 North Main Street

Image Info SH55001 Purves Street

Image Info SH55001 Route 83

Image Info SH55001 Westwood Avenue



The Commonwealth of Massachusetts

PLAN OF ROAD

in the Town of

EAST LONGMEADOW

HAMPDEN COUNTY

Altered and laid out as a State Highway by the
Department of Public Works

Scale : 40 feet to the inch

Chief Engineer

Layout N° 5402

This certifies that the location of the highway has been altered as shown on this plan and that said highway as altered was laid out and taken charge of as a State Highway by the Department of Public Works on _____ in accordance with Chapter 81 of the General Laws.

Department of Public Works

This certifies that the sections of road marked on this plan Town Layout were laid out in behalf of the Town of East Longmeadow by the Massachusetts Department of Public Works on _____ under authority of Chapter 448 of the Acts of 1948.

Department of Public Works



The Commonwealth of Massachusetts

Department of Public Works

100 Nashua Street, Boston 14

East Longmeadow
Layout No. 5402

July 15, 1964

081-12
081-6

Edward G. Shea, Clerk
Hampden County Commissioners
Court House
Springfield, Massachusetts

Dear Sir:

Acting under the provisions of Chapter 81 of the General Laws, Ter. Ed., the Department of Public Works did, under date of July 14, 1964, alter the location of a section of State highway laid out in East Longmeadow in the years 1904, 1906, and 1910.

A plan thereof and a certificate that said Department of Public Works has laid out and taken charge and has laid out said sections of highway in behalf of the Town of East Longmeadow of said State highway as altered in accordance with said plan, are sent you herewith - ~~under separate cover~~ - for filing in your office according to the law. An attested copy of the adjudication and vote of the Commissioners relative to the laying out and taking charge of said State highway and the laying out of said sections of highway in behalf of the Town of East Longmeadow as altered in accordance with said plan is enclosed.

Kindly sign and return to this office as soon as possible the enclosed form of receipt.

Yours truly,

Edith J. Cronin

Department Secretary.

~~Registered Mail~~
Enclosures

THE COMMONWEALTH OF MASSACHUSETTS

DEPARTMENT OF PUBLIC WORKS

--ooOoo--

Layout No. 5402
And Order of Taking

Whereas, the Massachusetts Highway Commission, acting on behalf of the Commonwealth of Massachusetts, did, under dates of March 10, 1904, May 1, 1906, and March 25, 1910, lay out and take charge of as a State highway, a road in the Town of East Longmeadow, County of Hampden being known as North Main St. (Route 83), and the Department of Public Works, acting on behalf of said Commonwealth, did under date of Nov. 15, 1927, alter portions of the location of said State highway as shown on the plans of said State highway on file in the office of the Department of Public Works, copies of which plans have been filed in the office of the County Commissioners of said County of Hampden at Springfield and in the office of the Town clerk of said Town of East Longmeadow; and

Whereas, it now appears advisable to make further changes in the location lines of said State highway;

Now, Therefore, acting under the provisions of Chapter 81 of the General Laws as appearing in the Tercentenary Edition thereof, and acts in amendment thereof and in addition thereto, the Department of Public Works adjudges that public necessity and convenience require that the Commonwealth should alter the location of said State highway, and should lay out and take charge of said State highway as altered, as hereinafter described:

The alteration is located on North Main St. (Route 83) and consists of widening, on the northeasterly side thereof, a portion of the State highway laid out on May 1, 1906 and on March 25, 1910 and of widening

on the southwesterly side thereof a portion of the State highway as laid out on May 1, 1906 and altered on Nov. 15, 1927 being more fully described as follows:

The base line of location of the State highway hereby altered and laid out is that of a survey made by Engineers of the Department in 1957 and begins at a point in the existing roadway of North Main St. shown on plan as Station 31+50.00 and extends thence south $65^{\circ} 42' 43''$ east, 513.32 ft.; thence south $58^{\circ} 33' 40''$ east, 574.31 ft.; thence south $62^{\circ} 37' 43''$ east, 482.85 ft.; thence south $45^{\circ} 36' 03''$ east, 182.92 ft.; thence south $17^{\circ} 57' 33''$ east, 187.60 ft. to a point of ending shown on plan as Station 50+91.00; then begins again at a point shown on plan as Station 68 and extends ^{thence} south $26^{\circ} 06' 03''$ east, 170.85 ft.; thence south $27^{\circ} 16' 03''$ east, 129.15 ft. to the point of ending thereof shown on plan as Station 71.

The northeasterly location line of the State highway hereby altered and laid out begins at a point on the northeasterly location line of the March 25, 1910 State highway layout bearing north $24^{\circ} 17' 17''$ east and 27.25 ft. distant from Station 31+50.00 of the base line hereinbefore described and extends thence, leaving said location line, south $66^{\circ} 37' 55''$ east, 597.32 ft. to a point bearing north $31^{\circ} 26' 20''$ east and 47.00 ft. distant from Station 37+42.00 of said base line; thence south $62^{\circ} 33' 41''$ east, 882.42 ft. to a point bearing north $27^{\circ} 22' 17''$ east and 81.00 ft. distant from Station 46+29.00 of said base line; thence south $40^{\circ} 25' 37''$ east, 286.22 ft. to a point at the end of the alteration on the northeasterly location line of the May 1, 1906 State highway layout, said point bearing north $24^{\circ} 21' 27''$ east and being 26.45 ft. distant from Station 49+03.40 of said base line.

The southwesterly location line of the State highway hereby altered and laid out begins at the point of beginning of the second section of

the Nov. 15, 1927 State highway alteration, said point bearing south $27^{\circ} 22' 17''$ west and being 24.75 ft. distant from Station 46+07.64 of the base line hereinbefore described and extends thence, leaving said location line, by a curve to the right of 543.73 ft. radius, 239.80 ft. to a point bearing south $44^{\circ} 23' 57''$ west and 38.49 ft. distant from Station 48+57.00 of said base line; thence not tangent to the line back, south $28^{\circ} 30' 30''$ east, 129.03 ft. to a point bearing south $72^{\circ} 02' 27''$ west and 32.00 ft. distant from Station 50+07.00 of said base line; thence south $22^{\circ} 53' 34''$ east, 84.31 ft. to a point at the end of the alteration on the southwesterly location line of the May 1, 1906 State highway layout, said point bearing south $72^{\circ} 02' 27''$ west and being 24.75 ft. distant from Station 50+91.00 of said base line.

The location lines of the State highway, altered and laid out as hereinbefore described, are to be further defined by bounds set thereon at angle points, at a point of curvature and at the points of beginning and ending thereof.

In connection with the laying out of the State highway as hereinbefore described, it is necessary to lay out two sections of highway on behalf of the Town of East Longmeadow and said sections of highway, are hereby so laid out on behalf of said Town of East Longmeadow, under the provisions of Chapter 448 of the Acts of 1948, being described as follows:

The first section of town highway hereby laid out on behalf of the Town of East Longmeadow is located on Westwood Ave. and consists of widening a portion of said avenue, on both sides thereof, adjoining the southwesterly location line of the March 10, 1904 State highway layout, being more fully described as follows:

The northerly location line of the first section of highway hereby

laid out on behalf of the Town of East Longmeadow begins at a point on the southwesterly location line of the March 10, 1904 State highway layout, said point bearing north $48^{\circ} 57' 04''$ west and being 63.74 ft. distant from Station 69+29.60 of the base line hereinbefore described and extends thence, leaving said location line, south $2^{\circ} 22' 52''$ west, 37.84 ft. to the point of ending thereof, on the existing northerly street line of Westwood Ave., said point bearing north $85^{\circ} 20' 03''$ west and being 49.80 ft. distant from said Station 69+29.60.

The southerly location line of the first section of highway hereby laid out on behalf of the Town of East Longmeadow begins at a point on the existing southerly street line of Westwood Ave., said point bearing north $85^{\circ} 55' 03''$ west and being 39.98 ft. distant from Station 69+84.90 of the base line hereinbefore described, and extends thence, leaving said street line, south $52^{\circ} 44' 12''$ east, 21.85 ft. to the point of ending thereof, on the southwesterly location line of the March 10, 1904 State highway layout, said point bearing south $65^{\circ} 13' 32''$ west and being 24.77 ft. distant from said Station 69+84.90.

The second section of highway hereby laid out on behalf of the Town of East Longmeadow is located on Maple Shade Ave. and consists of widening a portion of said avenue, on both sides thereof, at and adjacent to the northeasterly location line of the March 10, 1904 State highway layout, being more fully described as follows:

The northerly location line of the second section of highway hereby laid out on behalf of the Town of East Longmeadow begins at a point on the northeasterly location line of the March 10, 1904 State highway layout, said point bearing north $27^{\circ} 40' 25''$ east and being 33.96 ft. distant from Station 69+29.60 of the base line hereinbefore described and extends thence, leaving said location line easterly by a curve to the left of 50.00 ft. radius, 57.00 ft. to the point of ending thereof

on the existing northerly street line of Maple Shade Ave., said point bearing north $89^{\circ} 53' 22''$ east and being 60.65 ft. distant from said Station 69 + 29.60.

The southerly location line of the second section of highway hereby laid out on behalf of the Town of East Longmeadow begins at a point on the existing southerly street line of Maple Shade Ave., said point bearing south $86^{\circ} 40' 27''$ east and being 51.37 ft. distant from Station 69+84.90 of the base line hereinbefore described and extends thence, leaving said street line, southwesterly to southerly to southeasterly by a curve of 25.00 feet radius 33.69 ft. to the point of ending thereof, on the northeasterly location line of the March 10, 1904 State highway layout, said point bearing south $53^{\circ} 21' 52''$ east and being 56.26 ft. distant from said Station 69+84.90.

The location lines of the two sections of highway hereby laid out on behalf of the Town of East Longmeadow are to be further defined by bounds set thereon at the points of beginning and ending thereof.

For the purpose of laying out, constructing and maintaining said State highway, the Department of Public Works, on behalf of the Commonwealth of Massachusetts, does hereby take, under the provisions of Chapter 79 of the General Laws and of Chapter 718 of the Acts of 1956 as amended, all of the land outside of the limits of the existing public way or ways and within the limits of the layout hereinbefore described, including all trees and structures (not, however, including poles, towers, lines, wires, cables and other appurtenances for the conveyance of electricity and telephone communication) located thereon, situated in the Town of East Longmeadow, County of Hampden, all of said land being taken in fee simple, the supposed owners of Parcels 1 to 6, inclusive, shown on the plan hereinafter referred to, being set forth in the schedule hereinafter contained, excepting from the rights herein taken all easements for wires, pipes, conduits, poles

and other appurtenances for the conveyance of water, sewage, gas, oil and electricity and for telephone communication now lawfully in or upon said premises hereby taken and all lawful rights of the public to use those parts of the public way or ways in the Town of East Longmeadow which are included in the foregoing description.

For the purpose of laying out constructing and maintaining the sections of Town highway hereinbefore described, the Department of Public Works, on behalf of the Town of East Longmeadow, does hereby take, in fee simple, under the provisions of Chapter 79 of the General Laws, of Chapter 718 of the Acts of 1956 as amended Parcels 1-T to 4-T inclusive, shown on the plan hereinafter referred to, including all trees and structures (not, however, including poles, towers, lines, wires, cables and other appurtenances for the conveyance of electricity and telephone communication) located thereon, situated in the Town of East Longmeadow, County of Hampden, excepting from the rights herein taken all easements for wires, pipes, conduits, poles and other appurtenances for the conveyance of water, sewage, gas, oil and electricity and for telephone communication now lawfully in or upon said premises hereby taken and all lawful rights of the public to use those parts of the public way or ways in the Town of East Longmeadow which are included in the foregoing description.

The State highway hereby altered and laid out, the sections of Town highway hereby laid out and the aforesaid takings are shown on a plan signed by Daniel S. Horgan Chief Engineer and signed by the Department of Public Works and on file in its office, said plan being entitled "The Commonwealth of Massachusetts Plan of Road in the Town of East Longmeadow Hampden County Altered and laid out as State Highway by the Department of Public Works July 14, 1964 Scale: 40 feet to the inch" an attested copy of which is to be recorded with this order of layout and taking in the Registry of Deeds for Hampden County at Springfield.

The aforesaid takings are made for the purpose of a State highway or Town way or for ditches or drains for draining the same, and as provided by Chapter 79, Section 3, title to the fee of the property taken or to such other interest therein as has been designated in this order shall not vest in the Commonwealth or the Town nor shall the right to damages vest until such way, drain or ditch has been entered upon or possession thereof made, and if such entry is not made or possession taken within two years from this date the taking shall be void.

For damages sustained by the following persons in their property by reason of the aforesaid takings, and in accordance with the provisions of General Laws, Chapter 79, Section 6, as amended, awards are made. The Department reserves the right to amend the award or to increase the amount of damages to be paid at any time prior to the payment thereof for good cause shown.

<u>Parcel No.</u>	<u>Supposed Owner</u>	<u>Area</u>
1	A. & W. Development Corporation	90 S. F.
2	Eli & Anne Ross, Leonard & Annette Fastenberg, Hyman D. & Selma Rothkopf	4060 S. F.
3	Town of East Longmeadow	1.08 Acres
4	Loren H. Clark	380 S. F.
5	Pasquale & Restiva Rettura	550 S. F.
6	Town of East Longmeadow (Pleasantview School)	45 S. F.
1-T	Emily V. Speight	290 S. F.
2-T	Eugene J. Nowak and Robert F. Price	170 S. F.
3-T	United Petroleum Incorporated	65 S. F.
4-T	Thelma Ratner	235 S. F.

The names of owners herein given, although supposed to be correct, are such only as matters of opinion and belief.

It is therefore

Voted: that said way as altered, as described herein and as described and shown on said plan, be and the same is hereby laid out and taken charge of by the Commonwealth; that the Secretary of the Department of Public Works be and hereby is instructed to file, in the office of the County Commissioners of the County and in the office of the Town Clerk of the Town in which said way is located, certified copies of said plan and of a certificate, signed by at least a majority of the members of the Department of Public Works, attesting that said Department of Public Works has laid out and taken charge of said way as altered in accordance with said plan together with a copy of this adjudication and vote.

It is also

Voted: that the Secretary of the Department of Public Works be and hereby is instructed to file, in the office of the County Commissioners of the County and in the office of the Town Clerk of the Town in which said way is located, certified copies of said plan and of a certificate, signed by at least a majority of the members of the Department of Public Works, attesting that said Department of Public Works has laid out said sections of Town way in accordance with said plan.

Dated at Boston this fourteenth day of July 1964

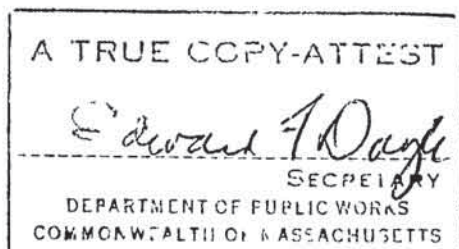
James D. Fitzgerald Department

John D. Warner of

D. R. Dwight Public Works

A. C. Rosselli

F. W. Sargent



1964

East Longmeadow



STATE HIGHWAY LAYOUT #5443
Route 83, North Main St., bridge alteration over
NYNH&H Railroad tracks

INDEXING

STATE HIGHWAY LAYOUT #5443

- Image Info SH55002 East Longmeadow
- Image Info SH55002 Bridge
- Image Info SH55002 New York, New Haven & Hartford Railroad
- Image Info SH55002 North Main Street
- Image Info SH55002 Route 83

EAST LONGMEADOW
NORTH MAIN ST.
1964 DECREE PLAN
SHEET 1 OF 2 SHEET

THE COMMONWEALTH OF MASSACHUSETTS
PLANS AND PROFILE SHOWING
PROPOSED ALTERATION OF BRIDGE IN THE TOWN OF

EAST LONGMEADOW
HAMPDEN COUNTY

NORTH MAIN STREET STA. 26+60.61

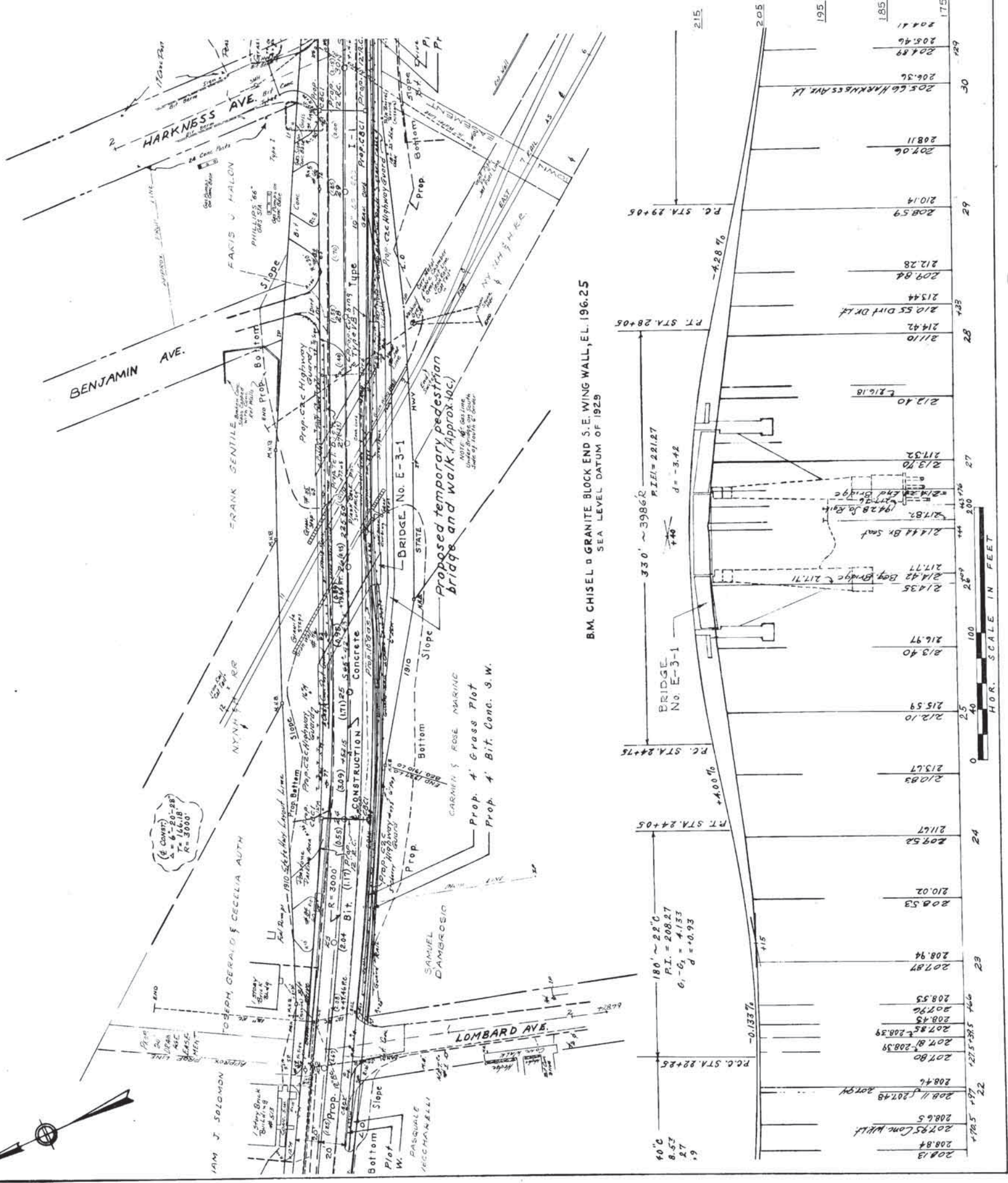
EAST HARTFORD TO SPRINGFIELD LINE OF

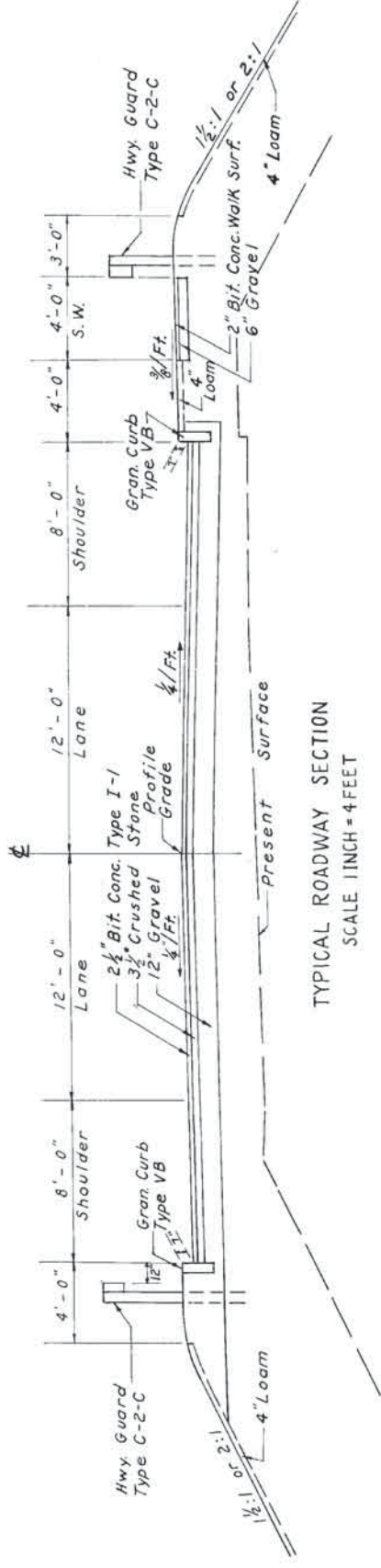
SCALES AS NOTED
DEPARTMENT OF PUBLIC WORKS
MARCH, 1964

APPROVED NOVEMBER 18 1964

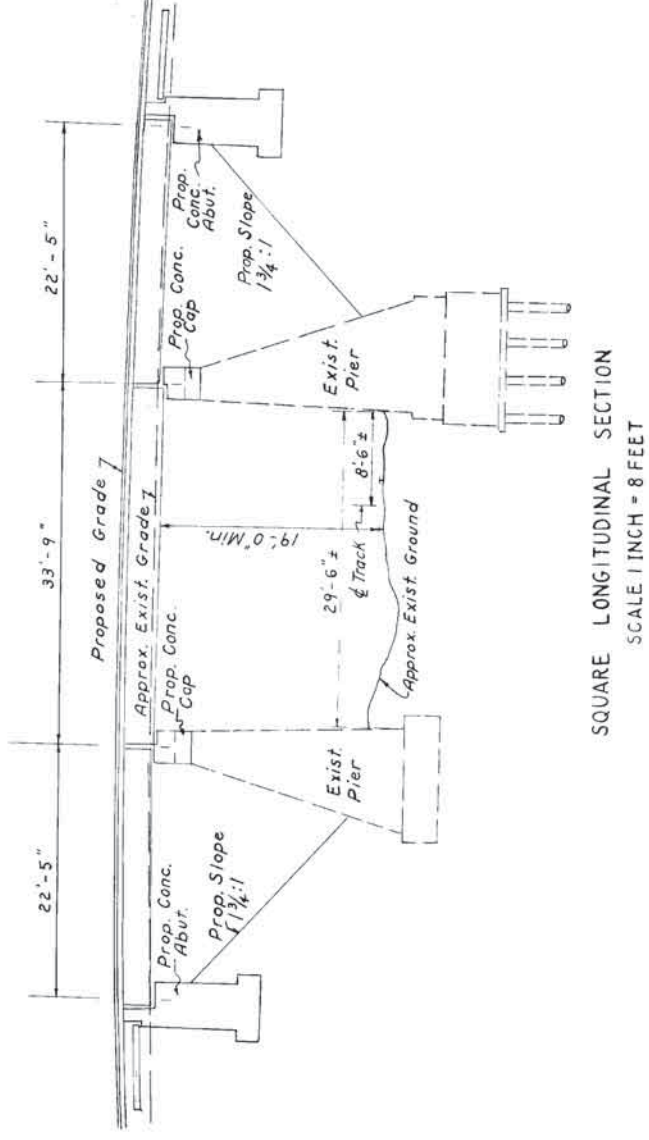
COMMISSIONER
ASSOCIATE
COMMISSIONER

A TRUE COPY ATTEST
Edward Folger
TREASURER, PUBLIC DEBTS COMMISSION
DEPARTMENT OF PUBLIC
COMMODITIES OF MASS. AUGUST 25TH 1891

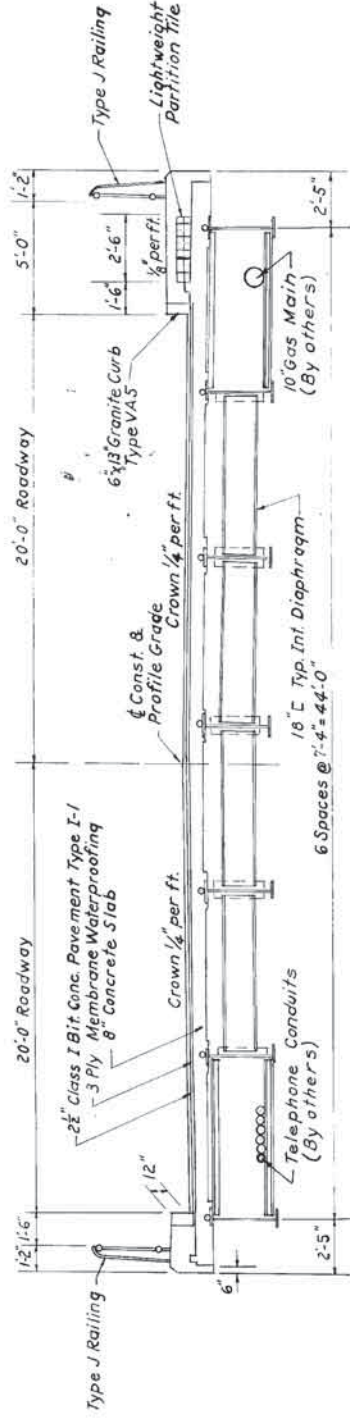




TYPICAL ROADWAY SECTION
SCALE 1 INCH = 4 FEET



SQUARE LONGITUDINAL SECTION
SCALE 1 INCH = 8 FEET



TYPICAL BRIDGE SECTION
SCALE 1 INCH = 4 FEET

THE COMMONWEALTH OF MASSACHUSETTS

DEPARTMENT OF PUBLIC WORKS

---oo0oo---

No. 5443

WHEREAS, a State highway in the Town of East Longmeadow, County of Hampden, known as North Main Street, crosses the East Hartford to Springfield line of the New York, New Haven and Hartford Railroad by an overhead bridge; and

WHEREAS, the Department of Public Works, acting under the provisions of Section 59, Chapter 159 of the General Laws, Ter. Ed., as amended, has considered the alteration of said bridge and has held a public hearing thereon at the Town Hall in East Longmeadow on February 27, 1964, due notice of which was given to all parties interested; and

WHEREAS, the Department of Public Works adjudges that public convenience and necessity require alterations in said bridge;

IT IS NOW ORDERED THAT

A new bridge shall be erected on North Main Street, Route 83, to replace the existing bridge over the East Hartford to Springfield Line of the New York, New Haven and Hartford Railroad Company; said bridge shall be a steel stringer three span structure with a reinforced concrete slab. The slab of said bridge shall carry a bituminous concrete surface forty feet wide with a granite curb and a five-foot concrete sidewalk on the southerly side and a granite curb and an eighteen-inch safety walk on the northerly side. The

-2-

span of the bridge over the railroad track shall have a minimum horizontal clearance between the northerly face of the southerly pier and the center line of the track of not less than eight feet and nine inches. The span over the track shall have a minimum vertical clearance over the top of the highest rail of not less than eighteen feet and six inches. Said bridge shall be designed in accordance with the 1961 edition of the Standard Specifications for Highways and Bridges and Incidental Structures of the American Association for Highway Officials for H-20-44 loading specified therein.

A temporary pedestrian bridge and walk shall be constructed on the southerly side of the New bridge during the construction of the new bridge and until the new bridge is open for travel. The type of temporary pedestrian bridge and walk to be the decision of the Department's Contractor.

The roadway pavement on North Main Street, Route 83, shall be constructed of bituminous concrete forty feet wide with granite curbing, a four-foot loam strip and a four-foot bituminous concrete sidewalk on the southerly side and granite curbing and a four-foot earth shoulder on the northerly side.

The center line of construction and finished grade on North Main Street, Route 83, are shown on plan hereinafter referred to. Highway guard rail shall be erected at all steep and dangerous places, granite curbing installed, cement concrete sidewalks and shoulders constructed and all work done substantially in accordance with the plan hereinafter referred to.

-3-

RAILROAD CHANGES

The grade at the base of the rail and the location of the railroad shall remain unchanged as at present. The poles, signals, wires and other appliances for the safe operation of the railroad shall be changed by the railroad to conform to the requirements of this order.

PUBLIC SERVICE STRUCTURES

All conduits, pipes, poles, wires or other property, with the exception of railroad property, located in public ways within the limits of the alterations hereby ordered shall be hereby relocated at such places and in such manner as the Department of Public Works may direct or otherwise cared for in accordance herewith.

IN GENERAL

Suitable drainage structures shall be constructed wherever required for proper drainage; said structures being shown on the plan hereinafter referred to.

PLANS

All of the above work shall be done substantially in accordance with a plan prepared by the Department of Public Works of the Commonwealth of Massachusetts entitled: "The Commonwealth of Massachusetts Plan and Profile Showing Proposed Alteration of Bridge in the Town of East Longmeadow Hampden County North Main St. Sta. 26+60.61 Over the East Hartford to Springfield Line of the New York, New Haven & Hartford Railroad Company Scales as Noted March 1964 Department of Public Works 100 Nashua St. Boston, Mass. C. F. Mistretta Chief Engineer", the original of which is on file in the office of the Department of Public Works.

In Witness Whereof we have set our hands this eighteenth day
of November in the year of our Lord, nineteen hundred and sixty-four.

[Signature]
[Signature]
[Signature]
[Signature]
[Signature]

Department
of
Public
Works

A TRUE COPY-ATTEST
Edward F. Doyle
SECRETARY, PUBLIC WORKS COMMISSION
DEPARTMENT OF PUBLIC WORKS
COMMONWEALTH OF MASSACHUSETTS

1986

East Longmeadow



STATE HIGHWAY LAYOUT #6742

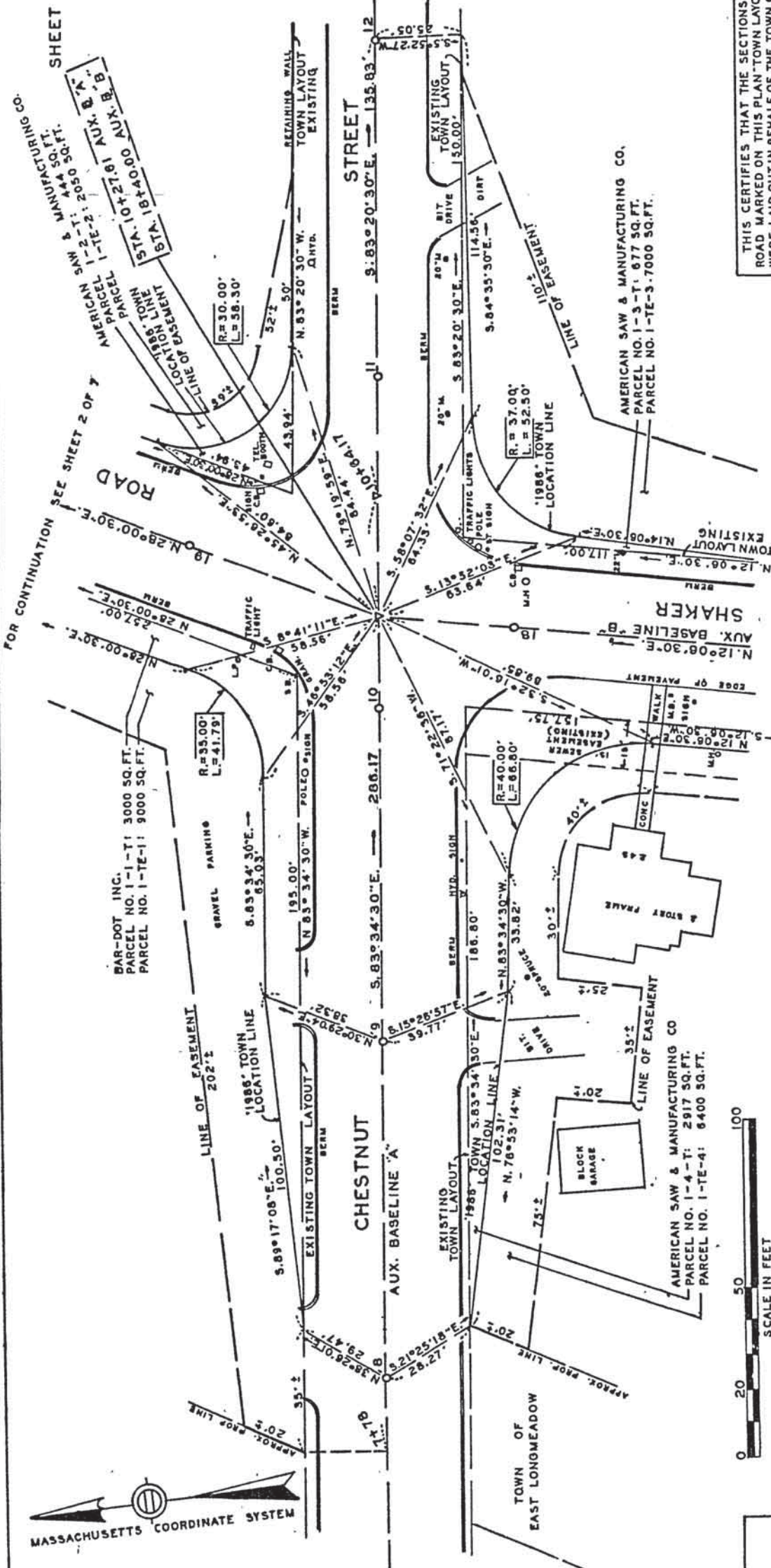
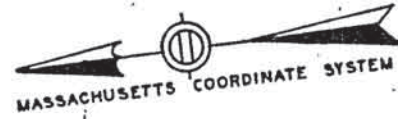
Intersections: North Main St., Allen St., Parker St., Chestnut St., Shaker Rd.

SEE HAMPDEN COUNTY REGISTRY OF DEEDS PLAN BOOK 235 PAGES 70-82.

INDEXING

STATE HIGHWAY LAYOUT #6742

- Image Info** SH55003 East Longmeadow
- Image Info** SH55003 Allen Street
- Image Info** SH55003 Chestnut Street
- Image Info** SH55003 North Main Street
- Image Info** SH55003 Parker Street
- Image Info** SH55003 Shaker Road



EAST-LONGMEADOW-1988 LAYOUT
SHEET 1 OF 7 - LOCATION 1 - LAYOUT No 6742
FEDERAL AID PROJECT No M0005(285)
SECTION 1

A TRUE COPY - ATTEST
Handwritten signature
SECRETARY, PUBLIC WORKS COMM.
DEPARTMENT OF PUBLIC WORKS
Commonwealth of Massachusetts

A TRUE COPY ATTEST
Handwritten signature
TOL. PRICE WORKS COMM.
DEPARTMENT OF PUBLIC WORKS
Commonwealth of Massachusetts



I CERTIFY THAT THIS PLAN HAS BEEN PREPARED
IN CONFORMITY WITH THE RULES AND REGULATIONS
OF THE REGISTER OF DEEDS OF THE COMMON-
WEALTH OF MASSACHUSETTS.

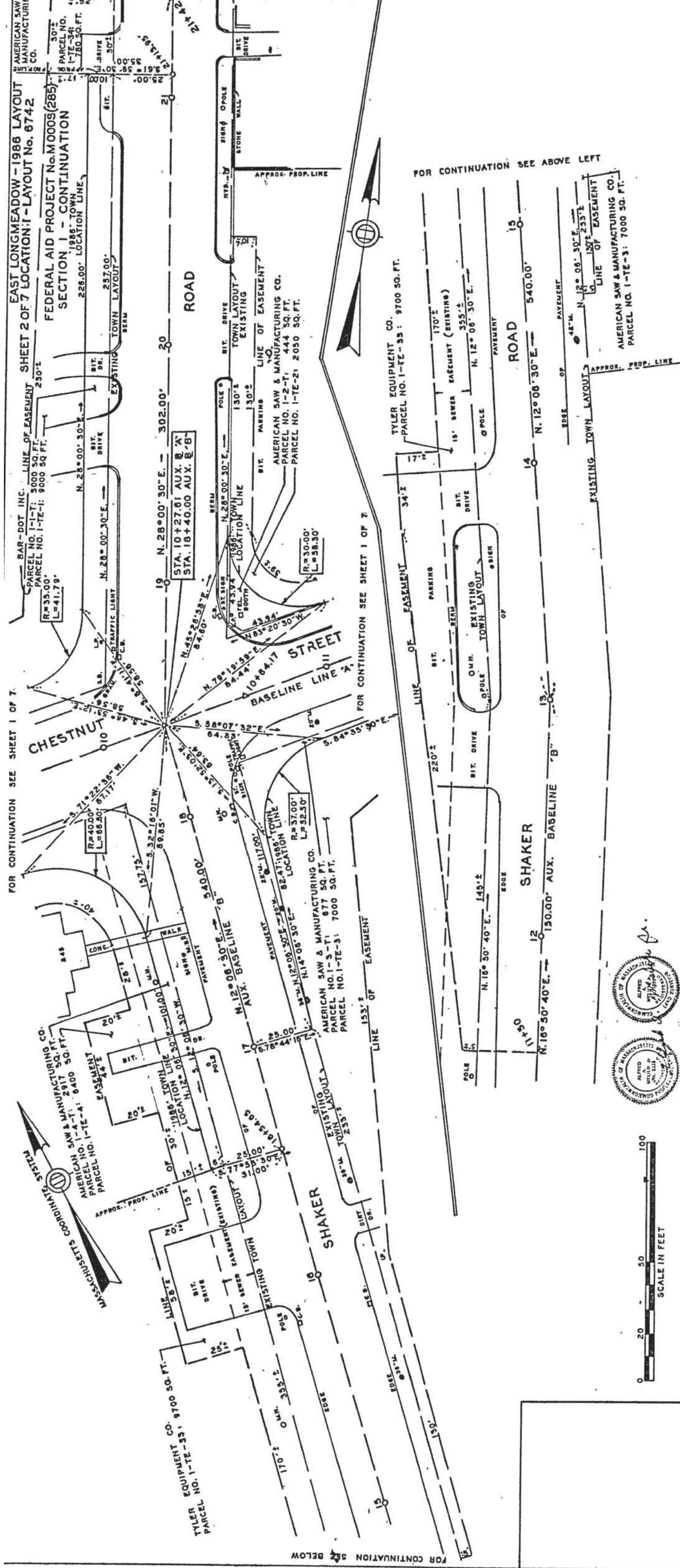
PLANS PREPARED BY
DEPARTMENT OF PUBLIC WORKS
EAST LONGMEADOW, MASS.

1/21/85
DATE
Handwritten signature
SIGNATURE

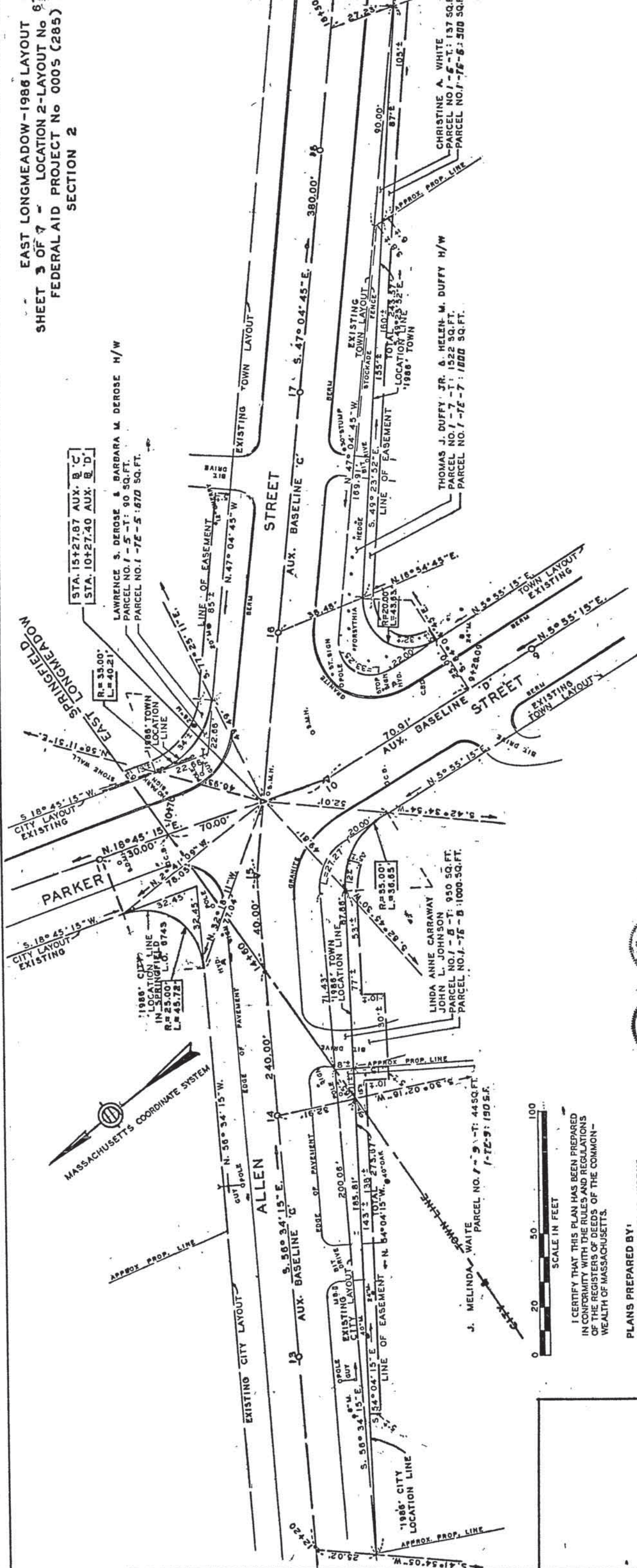
THIS CERTIFIES THAT THE SECTIONS OF
ROAD MARKED ON THIS PLAN "TOWN LAYOUT"
WERE LAID OUT IN BEHALF OF THE TOWN OF
EAST LONGMEADOW BY THE MASSACHUSETTS
DEPARTMENT OF PUBLIC WORKS ON APRIL
10, 1986 UNDER AUTHORITY OF CHAPTER 81
SECTION 29A OF THE GENERAL LAWS.

Handwritten signature
MASSACHUSETTS DEPARTMENT OF PUBLIC WORKS

THE COMMONWEALTH OF MASSACHUSETTS
PLAN OF ROAD
IN THE TOWN OF
EAST LONGMEADOW
HAMPDEN COUNTY
ALTERED AND LAID OUT AS A TOWN ROAD BY THE
DEPARTMENT OF PUBLIC WORKS
APRIL 10, 1986
SCALE = 40 FEET TO THE INCH
Handwritten signature
CHIEF ENGINEER
LAYOUT NO. 6742



EAST LONGMEADOW-1986 LAYOUT
SHEET 3 OF 7 - LOCATION 2-LAYOUT No 6
FEDERAL AID PROJECT No 0005 (285)
SECTION 2

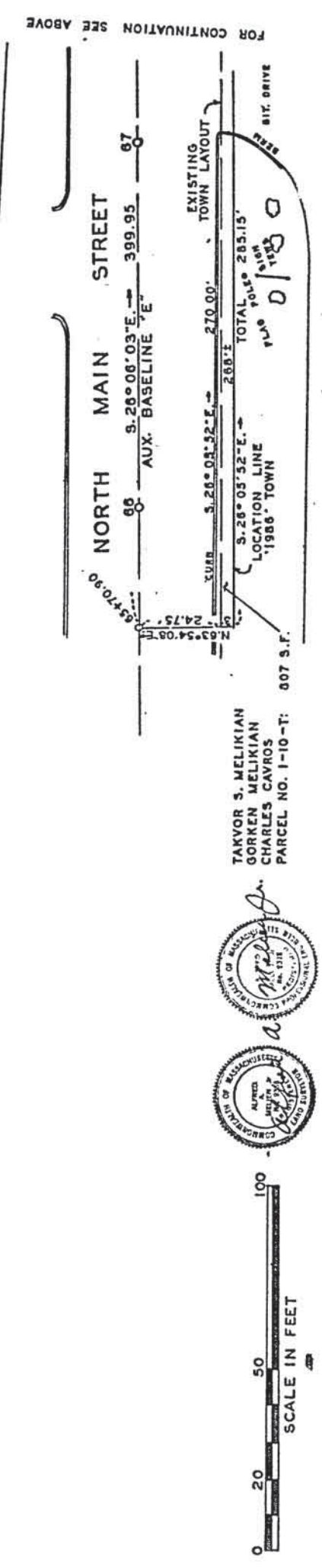
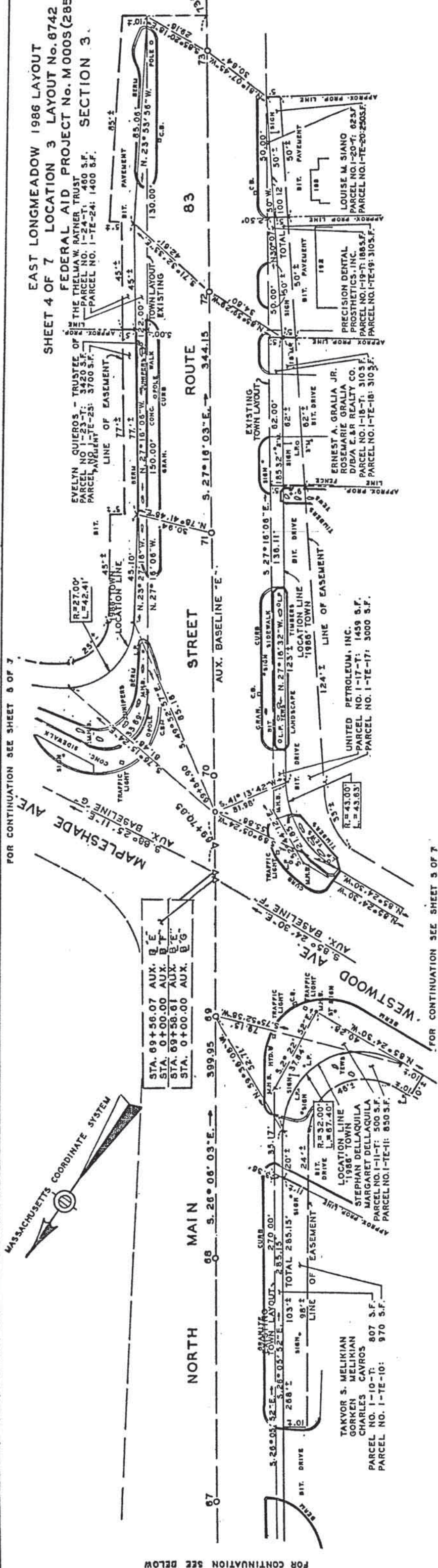


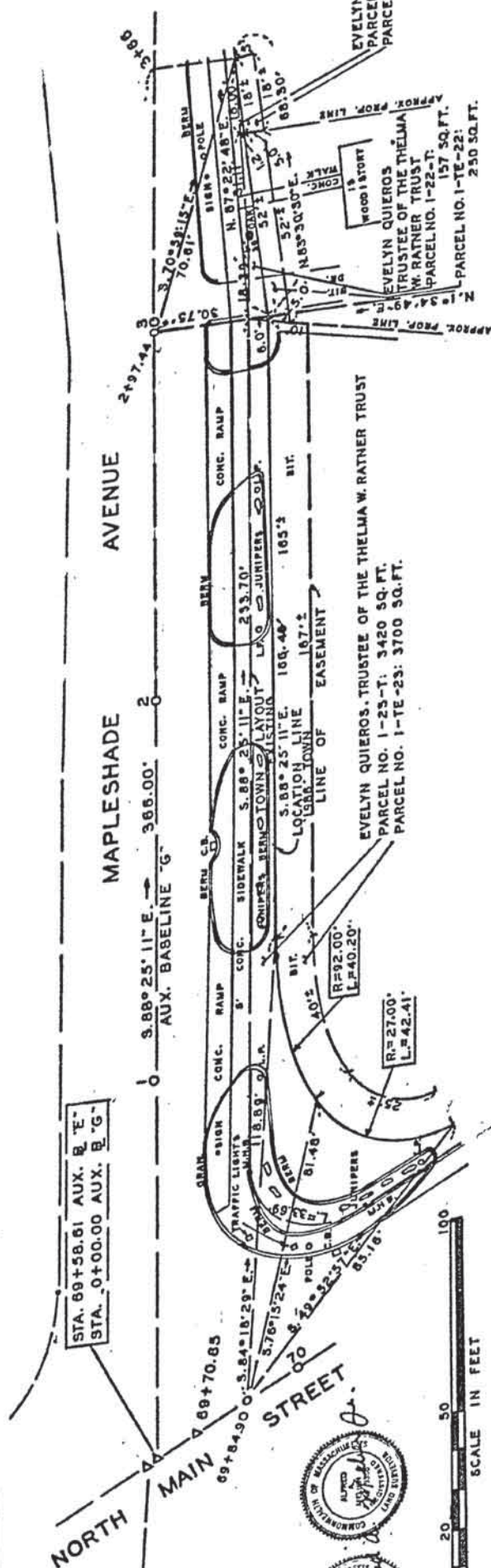
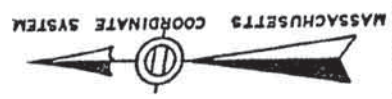
I CERTIFY THAT THIS PLAN HAS BEEN PREPARED
IN CONFORMITY WITH THE RULES AND REGULATIONS
OF THE REGISTERS OF DEEDS OF THE COMMON-
WEALTH OF MASSACHUSETTS.

PLANS PREPARED BY:
DEPARTMENT OF PUBLIC WORKS
EAST LONGMEADOW, MASS.

4-21-85 DATE
Alfred M. Melin, R.L.S. SIGNATURE







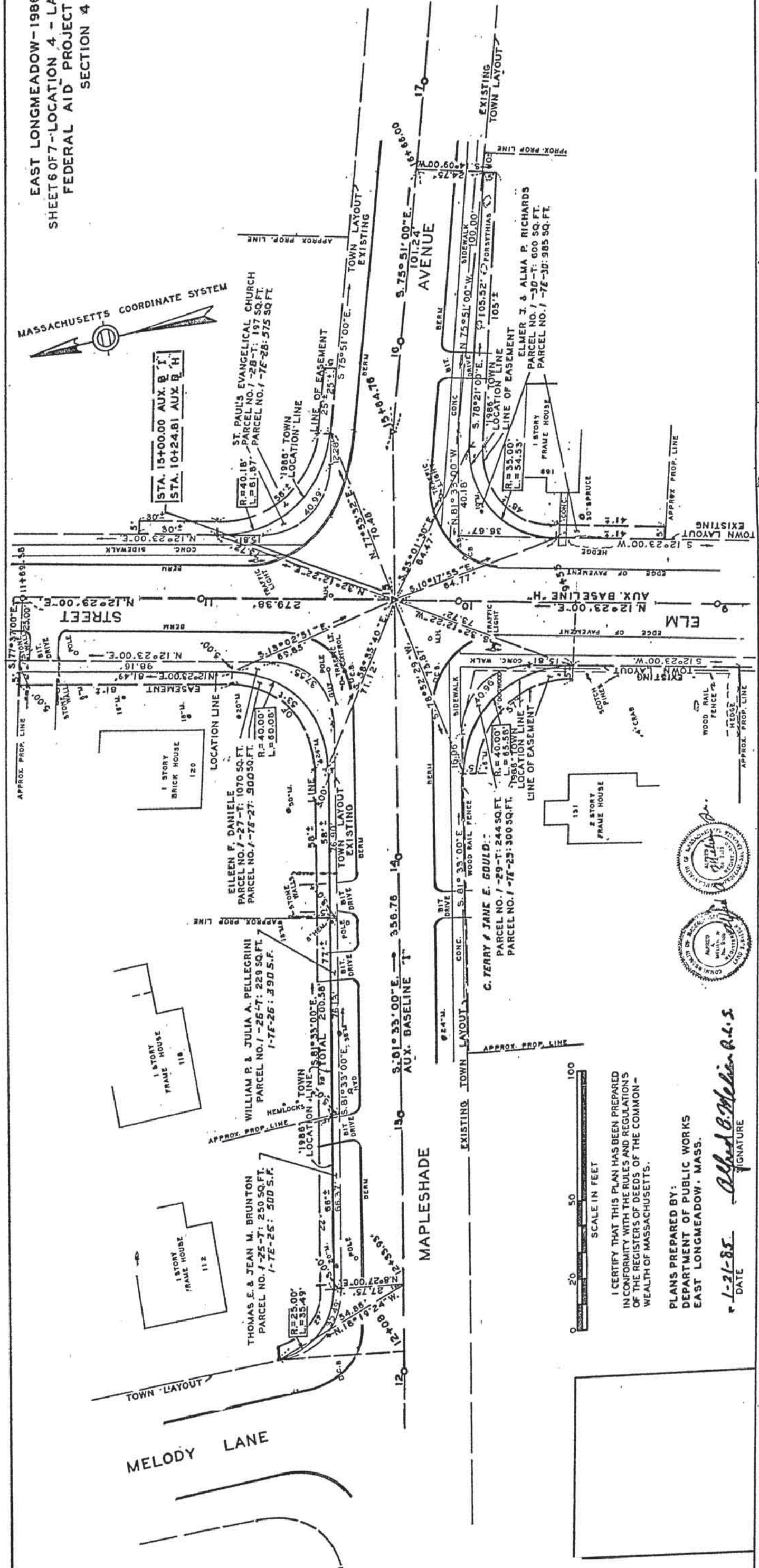
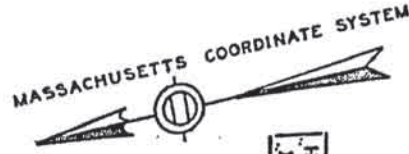
EVELYN QUIEROS, TRUSTEE OF THE THELMA W. RATNER TRUST
 PARCEL NO. 1-21-T: 11 SQ. FT.
 PARCEL NO. 1-TE-21: 100 SQ. FT.

MARYELYN QUIEROS, TRUSTEE OF THE THELMA W. RATHER TRUST
PARCEL NO. 1-25-T: 3420 SQ. FT.
PARCEL NO. 1-TE-23: 3700 SQ. FT.

83.18

APPROVED
BY
WILLIAM A. MILLER
No. 523
JAN 19 1964
NATIONAL PROFESSIONAL
COMMISSION

EAST LONGMEADOW - 1986 LAYOUT
 SHEET 6 OF 7 - LOCATION 4 - LAYOUT No. 674
 FEDERAL AID PROJECT No. M0005 (28)
 SECTION 4



I CERTIFY THAT THIS PLAN HAS BEEN PREPARED
 IN CONFORMITY WITH THE RULES AND REGULATIONS
 OF THE REGISTER OF DEEDS OF THE COMMON-
 WEALTH OF MASSACHUSETTS.

PLANS PREPARED BY:
 DEPARTMENT OF PUBLIC WORKS
 EAST LONGMEADOW, MASS.

DATE 1-21-85
 SIGNATURE *Alfred G. Melia R.L.S.*



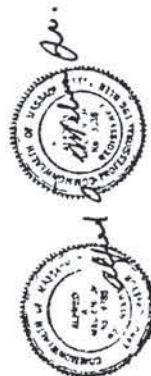
SECTION 5



I CERTIFY THAT THIS PLAN HAS BEEN PREPARED IN CONFORMITY WITH THE RULES AND REGULATION'S OF THE REGISTERS OF DEEDS OF THE COMMONWEALTH OF MASSACHUSETTS.

PLANS PREPARED BY:
DEPARTMENT OF PUBLIC WORKS
EAST-LONGMEADOW, MASS.

1-21-85
DATE-
Alfred A. Melina R.C.S.
SIGNATURE





The Commonwealth of Massachusetts

Executive Office of Transportation and Construction

Department of Public Works

Ten Park Plaza, Boston 02116

East Longmeadow L.O. 6742 dated

4-10-86 (Alteration)

Various Owners & Parcels

April 14, 1986

William J. Martin, Jr., Clerk
Hampden County Commissioners
Court House
Springfield, MA 01103

Dear Sir:

Acting under the provisions of Chapter 81 of the General Laws, Ter. Ed., the Department of Public Works did, under date of April 10, 1986 alter the location of a section of State highway laid out in the town of East Longmeadow in the year 1986

A plan thereof and a certificate that said Department of Public Works has laid out and taken charge of said State highway as altered in accordance with said plan; are sent you herewith for filing in your office according to the law. An attested copy of the adjudication and vote of the Commissioners relative to the laying out and taking charge of said State highway as altered in accordance with said plan is enclosed.

Kindly sign and return to this office as soon as possible the enclosed form of receipt.

Sincerely,

Joseph A. Fanale, Director
Right of Way Bureau

JAF/amb

Enclosure

"INTERSECTIONS"
=

Chestnut + Shaker
Allen + Parker
Maplebrook + No. Main
Westwood + No. Main
Maplebrook + Elm
Harkness + No. Main

The Commonwealth of Massachusetts

Layout No. 6742 dated
4-10-86

Hampden County

36

(Alteration)

April 30, 19 86

I HEREBY CERTIFY that the following document and plan have been this day filed in the office of the County Commissioners for said County, to wit.—

A certified copy of a plan and location bearing thereon a certificate dated April 10, 19 86, purporting to be signed by the Department of Public Works, or a majority of the members thereof, attesting that the said Department of Public Works has laid out and taken charge of, as a State highway, a new or existing way in the ~~city~~ town of East Longmeadow, as shown on said plan.

Attest

William J. Martin Jr.
Clerk of Courts.

Received by
Mary A. Solomon
Shelf Executive Asst to the
Hampden County Commissioners
April 30, 1986

REC'D COUNTY COMM.
APR 30 11 40 AM '86
COUNTY
OF
HAMPDEN

THE COMMONWEALTH OF MASSACHUSETTS

DEPARTMENT OF PUBLIC WORKS

East Longmeadow
Layout No. 6742
and Order of Taking

The Commonwealth of Massachusetts, Department of Public Works, acting under the authority of Chapter 81, Section 29A of the General Laws, and of all other general or special laws thereto enabling, hereby adjudges that public necessity and convenience require that said Department of Public Works should alter and lay out in the behalf of the Town of East Longmeadow portions of Chestnut Street, Shaker Road, Allen Street, Parker Street, North Main Street, Mapleshade Avenue, Westwood Avenue, Elm Street, and Harkness Avenue in the Town of East Longmeadow, County of Hampden.

The Alteration is made into five sections.

SECTION I

Auxiliary baseline "A" begins at a point within the existing layout of Chestnut Street, shown on said plan as Station 7 + 78 and extends thence South $83^{\circ}-34'-30''$ East 286.17 feet; thence South $83^{\circ}-20'-30''$ East 135.83 feet to the point of ending thereof shown on said plan as Station 12+00.

Auxiliary baseline "B" begins at a point within the existing layout of Shaker Road shown on said plan as Station 11+50 and extends thence North $16^{\circ}-50'-40''$ East 150.00 feet; thence North $12^{\circ}-06'-30''$ East 540.00 feet, thence North $28^{\circ}-00'-30''$ East 302.00 feet to the point of ending thereof shown on said plan as Station 21+42.

SECTION 2

Auxiliary baseline "C" begins at a point within the existing layout of Allen Street shown on said plan as Station 14+60 at the Springfield and East Longmeadow Town line and extends thence South $56^{\circ}-34'-15"$ East 40.00 feet, thence South $47^{\circ}-04'-45"$ East 380.00 feet to the point of ending thereof shown on said plan as Station 18+50.

Auxiliary baseline "D" begins at a point within the existing layout of Parker Street shown on said plan as Station 9+29.09 and extends thence North $5^{\circ}-55'-15"$ East 70.91 feet thence North $18^{\circ}-45'-15"$ East 70.00 feet to the Springfield and East Longmeadow Town line being the point of ending thereof shown on said plan as Station 10+70.

SECTION 3

Auxiliary baseline "E" begins at a point within the existing layout of North Main Street shown on said plan as Station 65+70.90 and extends thence South $26^{\circ}-06'-03"$ East 399.95 feet, thence South $27^{\circ}-16'-03"$ East 344.15 feet to the point of ending thereof shown on said plan as Station 73+15.

Auxiliary baseline "F" begins at a point within the existing layout of North Main Street and Westwood Avenue shown on said plan as Station 0+00 and extends Westerly North $85^{\circ}-24'-30"$ West 485.00 feet to the point of ending thereof shown on said plan as Station 4+85.

Auxiliary baseline "G" begins at a point within the existing layout of North Main Street and Mapleshade Avenue shown on said plan as Station 0+00 and extends Easterly South $88^{\circ}-25'-11"$ East 366.00 feet to the point of ending thereof shown on said plan as Station 3+66.

SECTION 4

Auxiliary baseline "H" begins at a point within the existing layout of Elm Street shown on said plan as Station 9+55 and extends North $12^{\circ}-23'-00''$ East 279.38 feet to the point of ending thereof shown on said plan as Station 11+69.38.

Auxiliary baseline "I" begins at a point within the existing layout of Mapleshade Avenue shown on said plan as Station 12+08 and extends South $81^{\circ}-33'-00''$ East 356.76 feet, thence South $75^{\circ}-51'-00''$ East 101.24 feet to the point of ending thereof shown on said plan as Station 16+66.00.

SECTION 5

Auxiliary baseline "J" begins at a point within the existing layout of North Main Street and Harkness Avenue shown on said plan as Station 0+00 and extends Northerly North $1^{\circ}-32'-36''$ East 162.00 feet to the point of ending thereof shown on said plan as Station 1+62.

Auxiliary baseline "K" begins at a point within the existing layout of North Main Street shown on said plan as Station 29+30 and extends thence South $65^{\circ}-42'-43''$ East 365.00 feet to the point of ending thereof shown on said plan as Station 32+95.

In East Longmeadow

The first section of highway hereby laid out on behalf of the Town of East Longmeadow establishes the location for the widening of Chestnut Street and Shaker Road at the intersection of said public ways, and is more fully described as follows:

The Northwesternly corner of said first section of Town way is bounded by the existing Northerly street line of Chestnut Street, by the existing Northwesternly street line of Shaker Road and by the line described as follows:

Beginning at a point on the aforesaid Northerly street line of Chestnut Street, said point bearing North $38^{\circ}-26'-01''$ East and being 29.47 feet distant from Station 8+00 on auxiliary baseline

"A", thence South $89^{\circ}-17'-08''$ East a distance of 100.50 feet to a point, said point bearing North $30^{\circ}-29'-04''$ East and being distant 38.32 feet from Station 9+00 on auxiliary baseline "A", thence bearing South $83^{\circ}-34'-30''$ East a distance of 65.03 feet to a point, said point bearing North $46^{\circ}-53'-12''$ West and being distant 58.56 feet from Station 10+27.61 on auxiliary baseline "A" and Station 18+40.00 on auxiliary baseline "B" at the intersection of both baselines. Thence Northeasterly along the arc of a curve having a radius of 35.00 feet a distance of 41.79 feet to a point, said point bearing North $8^{\circ}-41'-11''$ West and distant 58.56 feet from the intersection of the above mentioned baselines. Thence North $28^{\circ}-00'-30''$ East a distance of 226.00 feet to a point bearing North $61^{\circ}-59'-30''$ West and 35.00 feet distant from Station 21+13.95 on auxiliary baseline "B"; thence South $61^{\circ}-59'-30''$ East 10.00 feet to the point of ending on the existing Northwesterly street line of Shaker Road, said point bearing North $61^{\circ}-59'-30''$ West and being 25.00 feet distant from said Station 21+13.95.

The Northeasterly corner of said first section of Town way is bounded by the existing Northerly street line of Chestnut Street, by the existing Northeasterly street line of Shaker Road and by the line more fully described as follows:

Beginning at a point on the aforesaid Northerly street line of Chestnut Street, said point bearing North $79^{\circ}-19'-59''$ East and being distant 84.44 feet from Station 10+27.61 on auxiliary baseline "A" and Station 18+40.00 on auxiliary baseline "B" at the intersection of both baselines. Thence Northwesterly along the arc of a curve having a radius of 30.00 feet a distance of 58.30 feet to the point of ending on the existing Northeasterly street line of Shaker Road said point bearing North $45^{\circ}-26'-53''$ East and being distant 84.60 feet from the intersection of the above men-

tioned baselines.

The Southeasterly corner of said first section of Town way is bounded by the existing Easterly street line of Shaker Road, by the existing Southerly street line of Chestnut Street and by the line more fully described as follows:

Beginning at a point on the aforesaid Easterly street line of Shaker Road, said point bearing South $78^{\circ}-44'-16''$ East and being distant 25.00 feet from Station 17+00 on auxiliary baseline "B", thence bearing North $14^{\circ}-06'-30''$ East a distance of 82.47 feet to a point, said point bearing South $13^{\circ}-52'-03''$ East and being distant 63.64 feet from Station 18+40.00 on auxiliary baseline "B" and Station 10+27.61 on auxiliary baseline "A" at the intersection of both baselines, thence Northeasterly along the arc of a curve having a radius of 37.00 feet a distance of 52.50 feet to a point, said point bearing South $58^{\circ}-07'-32''$ East and being distant 64.33 feet from the intersection of the above mentioned baselines, thence bearing South $84^{\circ}-35'-30''$ East a distance of 114.56 feet to a point at the end of taking on the existing Southerly street line of Chestnut Street said point bearing South $5^{\circ}-52'-27''$ West and being distant 25.05 feet from Station 12+00 on auxiliary baseline "A".

The Southwesterly corner of said first section of Town way is bounded by the existing Southerly street line of Chestnut Street, by the existing Westerly street line of Shaker Road, and by the line more fully described as follows:

Beginning at a point on the aforesaid Westerly street line of Shaker Road, said point bearing North $77^{\circ}-53'-30''$ West and distant 25.00 feet from Station 16+54.65 on auxiliary baseline "B", thence North $77^{\circ}-53'-30''$ West a distance of 6.00 feet to a point, thence

North $12^{\circ}-06'-30''$ East a distance of 101.00 feet to a point, said point bearing South $32^{\circ}-16'-01''$ West and distant 89.85 feet from Station 18+40.00 on auxiliary baseline "B" and Station 10+27.61 on auxiliary baseline "A" at the intersection of both baselines, thence Northwesterly along the arc of a curve having a radius of 40.00 feet a distance of 66.80 feet to a point, said point bearing South $71^{\circ}-22'-36''$ West and distant 87.17 feet from the intersection of the above mentioned baselines, thence bearing North $83^{\circ}-34'-30''$ West a distance of 33.82 feet to a point, said point bearing South $15^{\circ}-26'-57''$ East and distant 39.77 feet from Station 9+00 on auxiliary baseline "A", thence North $76^{\circ}-53'-14''$ West a distance of 102.31 feet to the point of ending on the existing Southerly street line of Chestnut Street, said point bearing South $21^{\circ}-25'-18''$ East and distant 28.27' from Station 8+00 on auxiliary baseline "A".

In East Longmeadow

The second section of highway hereby laid out on behalf of the Town of East Longmeadow establishes the location for the widening of Allen Street and Parker Street at the intersection of said public ways, and is more fully described as follows:

The Northeasterly corner of said second section of Town way is bounded by the existing Northerly street line of Allen Street, by the existing Easterly street line of Parker Street and by the line more fully described as follows:

Beginning at a point on the aforesaid Northerly street line of Allen Street, said point bearing South $77^{\circ}-25'-11''$ East and being distant 49.49 feet from Station 15+27.87 on auxiliary baseline "C" and Station 10+27.40 on auxiliary baseline "D" at the intersection of both baselines, thence Northwesterly along the arc of a curve having a radius of 35.00 feet a distance of

40.21 feet to the point of ending on the existing Easterly street line of Parker Street, said point bearing North $56^{\circ}-11'-51''$ East and distant 46.93 feet from the intersection of the above mentioned baselines.

The Southeasterly corner of said second section of Town way is bounded by the existing Easterly street line of Parker Street, by the existing Southerly street line of Allen Street and by the line more fully described as follows:

Beginning at a point on the aforesaid Easterly street line of Parker Street, said point bearing South $84^{\circ}-04'-45''$ East and being distant 25.00 feet from Station 9+29.09 on auxiliary baseline "D", thence Northeasterly along the arc of a curve having a radius of 20.00 feet a distance of 43.63 feet to a point, said point bearing South $16^{\circ}-54'-45''$ West and being distant 38.48 feet from Station 16+00 on auxiliary baseline "C", thence bearing South $49^{\circ}-23'-52''$ East a distance of 243.57 feet to the point of ending on the existing Southerly street line of Allen Street, said point bearing South $20^{\circ}-46'-33''$ West and being distant 27.23 feet from Station 18+50 on auxiliary baseline "C".

The Southwesterly corner of said second section of Town way is bounded by the existing Westerly street line of Parker Street, by the existing Southerly street line of Allen Street and by the line more fully described as follows:

Beginning at a point on the aforesaid Westerly street line of Parker Street, said point bearing South $42^{\circ}-34'-54''$ West and being distant 52.01 feet from Station 15+27.87 on auxiliary baseline "C" and Station 10+27.40 on auxiliary baseline "D" at the intersection of both baselines, thence Northwesterly along the arc of a curve having a radius of 35.00 feet a distance of 36.65 feet to a point, said point bearing South $82^{\circ}-45'-30''$ West and being distant 49.81 feet from the

intersection of the above mentioned baselines. Thence bearing North $54^{\circ}-04'-15''$ West a distance of 87.86 feet to the Springfield line at the point of ending said point bearing South $30^{\circ}-02'-16''$ West and distant 32.91 feet from Station 14+00 on auxiliary baseline "C".

In East Longmeadow

The third section of highway hereby laid out on behalf of the Town of East Longmeadow establishes the location for the widening of North Main Street, Westwood Avenue and Mapleshade Avenue at the intersection of said public ways and is more fully described as follows:

The Northwesternly corner of said third section of Town way is bounded by the existing Westerly street line of North Main Street, by the existing Northerly street line of Westwood Avenue and by the line more fully described as follows:

Beginning at a point on the aforesaid Westerly street line of North Main Street, said point bearing South $63^{\circ}-54'-08''$ West and being distant 24.75 feet from Station 65+70.90 on auxiliary baseline "E", thence South $63^{\circ}-54'-08''$ West a distance of 3.00 feet to a point, thence South $26^{\circ}-05'-52''$ East a distance of 285.15 feet to a point, said point bearing North $59^{\circ}-36'-08''$ West and being distant 52.71 feet from Station 69+00, thence Southwesterly along the arc of a curve having a radius of 32.00 feet a distance of 67.40 feet to the point of ending on the existing Northerly street line of Westwood Avenue and bearing South $75^{\circ}-52'-58''$ West and distant 79.15 feet from Station 69+00 on auxiliary baseline "E".

The Southwesterly corner of said third section of Town way is bounded by the existing Southerly street line of Westwood Avenue, by the Westerly street line of North Main Street and by the line more fully described as follows:

Beginning at a point on the aforesaid Southerly street line of Westwood Avenue said point bearing South $82^{\circ}-12'-50''$ West and being distant 115.46 feet from Station 3+73.96 on auxiliary baseline "F", thence South $82^{\circ}-52'-12''$ East a distance of 112.89 feet to a point, said point bearing South $4^{\circ}-35'-30''$ West and being distant 34.75 feet from Station 3+73.96 on auxiliary baseline "F", thence South $85^{\circ}-24'-30''$ East a distance of 333.32 feet to a

point, said point bearing South $89^{\circ}-05'-24''$ West and being distant 55.86 feet from Station 69+84.90 on auxiliary baseline "E", thence Southeasterly along the arc of a curve having a radius of 43.00 feet a distance of 43.63 feet to a point, said point bearing South $41^{\circ}-13'-42''$ West and being distant 31.98 feet from Station 69+84.90 on auxiliary baseline "E", thence South $27^{\circ}-16'-32''$ East a distance of 185.32 feet to a point, said point bearing North $85^{\circ}-59'-29''$ West and being distant 34.80 feet from Station 72+00 on auxiliary baseline "E", thence South $30^{\circ}-07'-50''$ East a distance of 100.12 feet to the point of ending on the existing Westerly street line of North Main Street, said point bearing North $81^{\circ}-07'-45''$ West and being distant 30.64 feet from Station 73+00 on auxiliary baseline "E".

The Southeasterly corner of said third section of Town way is bounded by the existing Easterly street line of North Main Street, by the Southerly street line of Mapleshade Avenue, and by the line more fully described as follows:

Beginning at a point on the aforesaid Easterly street line of North Main Street, said point bearing South $85^{\circ}-20'-18''$ East and being distant 29.16 feet from Station 73+00 on auxiliary baseline "E", thence North $23^{\circ}-53'-56''$ West a distance of 85.06 feet to a point, said point bearing South $71^{\circ}-32'-33''$ East and being distant 42.61 feet from Station 72+00 on auxiliary baseline "E", thence bearing North $27^{\circ}-16'-06''$ West a distance of 122.00 feet to a point, said point bearing North $78^{\circ}-41'-46''$ East and being distant 30.94 feet from Station 71+00 on auxiliary baseline "E", thence North $23^{\circ}-27'-16''$ West a distance of 45.10 feet to a point, said point bearing South $49^{\circ}-52'-57''$ East and being distant 85.16 feet from Station 69+84.90 on auxiliary baseline "E", thence Northeasterly along the arc of a curve having a radius of 27.00 feet a distance of 42.41

feet to a point, said point bearing South $76^{\circ}-15'-24''$ East and being distant 81.48 feet from Station 69+84.90 on auxiliary baseline "E", thence Easterly along the arc of a curve having a radius of 92.00 feet, a distance of 40.20 feet to a point, said point bearing South $84^{\circ}-18'-29''$ East and being distant 118.89 feet from Station 69+84.90 on auxiliary baseline "E", thence bearing South $88^{\circ}-25'-11''$ East a distance of 166.46 feet to a point, said point bearing South $1^{\circ}-34'-49''$ West and being distant 30.75 feet from Station 2+97.44 on auxiliary baseline "G", thence bearing North $83^{\circ}-30'-30''$ East a distance of 68.30 feet to the point of ending on the existing Southerly street line of Mapleshade Avenue bearing South $70^{\circ}-59'-15''$ East and being distant 70.61 feet from Station 2+97.44 on auxiliary baseline "G".

In East Longmeadow

The fourth section of highway hereby laid out on behalf of the Town of East Longmeadow establishes a location for the widening of Elm Street, Melody Lane and Mapleshade Avenue at the intersection of said public ways, and is more fully described as follows:

The Northwesternly corner of said fourth section of Town way is bounded by the existing Easterly street line of Melody Lane, by the Northerly street line of Mapleshade Avenue, by the Westerly street line of Elm Street and by the line more fully described as follows:

Beginning at a point on the aforesaid Easterly street line of Melody Lane, said point bearing North $18^{\circ}-19'-24''$ West and being distant 54.86 feet from Station 12+33.93 on auxiliary baseline "I", thence Southeasterly along the arc of a curve having a radius of 25.00 feet a distance of 35.49 feet to a point, said point bearing North $8^{\circ}-27'-00''$ East and being distant 27.75 feet from Station 12+33.93 on auxiliary baseline "I", thence bearing South $81^{\circ}-33'-00''$ East a distance of 200.58 feet to a point, said point bearing North $58^{\circ}-35'-40''$ West and being distant 71.12 feet from Station 15+00.00 on auxiliary baseline "I" and Station 10+24.81 on auxiliary baseline "H" at the intersection of both baselines, thence

Northeasterly along the arc of a curve having a radius of 40.00 feet a distance of 60.08 feet to a point, said point bearing North $13^{\circ}-02'-51''$ West and being distant 69.85 feet from the intersection of the above mentioned baselines, thence bearing North $12^{\circ}-23'-00''$ East a distance of 81.49 feet to a point, thence South $77^{\circ}-37'-00''$ East a distance of 5.00 feet to the point of ending on the existing street line of Elm Street, said point bearing North $77^{\circ}-37'-00''$ West and being distant 25.00 feet from Station 11+69.38 on auxiliary baseline "H".

The Northeasterly corner of said forth section of Town way is bounded by the existing Northerly street line of Mapleshade Avenue, by the Easterly street line of Elm Street and by the line more fully described as follows:

Beginning at a point on the aforesaid Northerly street line of Mapleshade Avenue, said point bearing North $77^{\circ}-53'-32''$ East and being distant 70.48 feet from Station 15+00.00 on auxiliary baseline "I" and Station 10+24.81 on auxiliary baseline "H" at the intersection of both baselines, thence Northwesterly along the arc of a curve having a radius of 40.18 feet a distance of 61.87 feet to the point of ending on the existing Easterly street line of Elm Street, said point bearing North $32^{\circ}-12'-22''$ East and being distant 73.72 feet from the intersection of the above mentioned baselines.

The Southeasterly corner of said forth section of Town way is bounded by the existing Easterly street line of Elm Street, by the Southerly street line of Mapleshade Avenue and by the line more fully described as follows:

Beginning at a point on the aforesaid Easterly street line of Elm Street, said point bearing South $10^{\circ}-17'-55''$ East and being distant 64.77 feet from Station 10+24.81 on auxiliary baseline "H" and Station 15+00.00 on auxiliary baseline "I" at the intersection of

both baselines, thence Northeasterly along the arc of a curve having a radius of 35.00 feet a distance of 54.53 feet to a point, said point bearing South $55^{\circ}-01'-30''$ East and being distant 64.47 feet from the intersection of the above mentioned baselines, thence bearing South $78^{\circ}-21'-00''$ East a distance of 105.52 feet to the point of ending on the existing Southerly street line of Mapleshade Avenue, said point bearing South $14^{\circ}-09'-00''$ West and being distant 24.75 feet from Station 16+66.00 on auxiliary baseline "I".

The Southwesterly corner of said fourth section of Town way is bounded by the existing Westerly street line of Elm Street, by the Southerly street line of Mapleshade Avenue and by the line more fully described as follows:

Beginning at a point on the aforesaid Westerly street line of Elm Street, said point bearing South $32^{\circ}-12'-22''$ West and being distant 73.72 feet from Station 10+24.81 on auxiliary baseline "H" and Station 15+00.00 on auxiliary baseline "I" at the intersection of both baselines, thence Northwesterly along the arc of a curve having a radius of 40.00 feet a distance of 65.58 feet to the point of ending on the existing Southerly street line of Mapleshade Avenue, said point bearing South $78^{\circ}-52'-29''$ West and being distant 73.87 feet from the intersection of the above mentioned baselines.

In East Longmeadow

The fifth section of highway hereby laid out on behalf of the Town of East Longmeadow establishes a location for the widening of North Main Street and Harkness Avenue at the intersection of said public ways and is more fully described as follows:

The Northwesterly corner of said fifth section of Town way is bounded by the existing Northerly street line of North Main Street, by the Westerly street line of Harkness Avenue and by the line more fully described as follows:

Beginning at a point on the aforesaid Northerly street line of North Main Street, said point bearing North $39^{\circ}-17'-12''$ West and being distant 93.55 feet from Station 30+16.12 on auxiliary baseline "K" and Station 0+00 on auxiliary baseline "J" at the intersection of both baselines, thence Northeasterly along the arc of a curve having a radius of

25.00 feet a distance of 50.84 feet to the point of ending on the existing Westerly street line of Harkness Avenue, said point bearing South $76^{\circ}-13'-50''$ West and being distant 25.92 feet from Station 1+00 on auxiliary baseline "J" on Harkness Avenue.

The Northeasterly corner of said fifth section of Town way is bounded by the existing Northerly street line of North Main Street, by the Easterly street line of Harkness Avenue, and by the line more fully described as follows:

Beginning at a point on the aforesaid Northerly street line of North Main Street, said point bearing North $12^{\circ}-24'-27''$ East and being distant 31.21 feet from Station 33+00 on auxiliary baseline "K", thence bearing North $63^{\circ}-17'-27''$ West a distance of 243.36 feet to a point. Said point bearing North $65^{\circ}-12'-20''$ East and being distant 52.71 feet from Station 30+16.12 on auxiliary baseline "K" and Station 0+00 on auxiliary baseline "J", thence along the arc of a curve having a radius of 25.00 feet a distance of 26.98 feet to a point. Said point bearing North $37^{\circ}-54'-12''$ East and being distant 55.51 feet from the intersection of the above mentioned baselines. Thence North $1^{\circ}-27'-24''$ West a distance of 55.65 feet to the point of ending on the existing Easterly street line of Harkness Avenue, said point bearing South $88^{\circ}-59'-24''$ East and being distant 30.09 feet from Station 1+00 on auxiliary baseline "J".

The location lines of the sections of highway hereby laid out on behalf of the Town of East Longmeadow are to be further defined by bounds set thereon at angle points, points of curvature and at points of beginning and ending thereof, where feasible.

Easements are hereby taken in the parcels of land shown on the plan hereinafter referred to as parcels 1-TE-1, 1-TE-2, 1-TE-3, 1-TE-4, 1-TE-5, 1-TE-6, 1-TE-7, 1-TE-8, 1-TE-9, 1-TE-10, 1-TE-11, 1-TE-12, 1-TE-13, 1-TE-14, 1-TE-15, 1-TE-16, 1-TE-17, 1-TE-18, 1-TE-19,

1-TE-20, 1-TE-21, 1-TE-22, 1-TE-23, 1-TE-24, 1-TE-25, 1-TE-26, 1-TE-27, 1-TE-28, 1-TE-29, 1-TE-30, 1-TE-31, 1-TE-32, 1-TE-33, and 1-TE-34 inclusive for the purpose of effecting the necessary construction within said parcels, and consist of the right to enter upon said land at any time during the effective period of the easements to construct thereon slopes of excavation and/or embankment. Said easements are temporary in nature and are to be in effect only until three years from the date of this instrument.

The location of the aforesaid easements are more particularly shown on the plan hereafter referred to.

For the purpose of laying out, constructing and maintaining the sections of Town Street hereinbefore described, the Department of Public Works, on behalf of the Town of East Longmeadow, does hereby take in fee simple, under the provisions of Chapter 79 of the General Laws, and of all other general or special laws thereto enabling, Parcels 1-1-T, 1-2-T, 1-3-T, 1-4-T, 1-5-T, 1-6-T, 1-7-T, 1-8-T, 1-9-T, 1-10-T, 1-11-T, 1-12-T, 1-13-T, 1-14-T, 1-15-T, 1-16-T, 1-17-T, 1-18-T, 1-19-T, 1-20-T, 1-21-T, 1-22-T, 1-23-T, 1-24-T, 1-25-T, 1-26-T, 1-27-T, 1-28-T, 1-29-T, 1-30-T, 1-31-T, 1-32-T and 1-33-T shown on the plan hereinafter referred to, including all trees and structures located thereon (not, however, including poles, towers, lines, wires, cables and other appurtenances for the conveyance of electricity and telephone communication), situated in the Town of East Longmeadow, County of Hampden, excepting from the rights herein taken, all easements for wires, pipes, conduits, poles and other appurtenances for the conveyance of water, sewage, gas, oil and electricity and for telephone communication now lawfully in or upon said premises hereby taken, and all lawful rights of the public to use those parts of the public way or ways in the Town of East Longmeadow which are included

in the foregoing description.

The sections of The Town Road hereby laid out, and the aforesaid takings are shown on a plan signed by the Chief Engineer, and signed by the Department of Public Works and on file in its office, said plan being entitled: "The Commonwealth of Massachusetts Plan of Road in the Town of East Longmeadow, Hampden County Altered and laid out as a Town Road by the Department of Public Works April 10, 1986 Scale: 20 feet to the inch", an attested copy of which is to be recorded with this order of layout and taking in the Registry of Deeds of Hampden County, in Springfield.

For damages sustained by the following persons in their property by reason of the aforesaid takings, and in accordance with the provisions of General Laws. Chapter 79, Section 6, as amended, awards are made.

The Department reserves the right to amend the award anytime prior to the payment thereof for good cause shown.

<u>Parcel</u>	<u>Supposed Owner</u>	<u>Area</u>
1-1-T	Bardot-Inc.	3,000 s.f.
1-TE-1	" "	9,000 s.f.
1-2-T	American Saw & Manufacturing Co.	444 s.f.
1-TE-2	" "	2,050 s.f.
1-3-T	" "	677 s.f.
1-TE-3	" "	7,000 s.f.
1-4-T	" "	2,917 s.f.
1-TE-4	" "	6,400 s.f.
1-5-T	Lawrence S. Derosé & Barbara M. Derosé H/W	90 s.f.
1-TE-5	Lawrence S. Derosé & Barbara M. Derosé H/W	670 s.f.

<u>Parcel</u>	<u>Supposed Owner</u>	<u>Area</u>
1-6-T	Christine A. White	137 s.f.
1-TE-6	Christine A. White	500 s.f.
1-7-T	Thomas J. Duffy, Jr. & Helen M. Duffy H/W	1,522 s.f.
1-TE-7	Thomas J. Duffy, Jr. & Helen M. Duffy H/W	1,000 s.f.
1-8-T	John L. Johnson & Linda Anne Carraway	950 s.f.
1-TE-8	John L. Johnson & Linda Anne Carraway	1,000 s.f.
1-9-T	J. Melinda Waite	44 s.f.
1-TE-9	J. Melinda Waite	190 s.f.
1-10-T	Takvor S. Melikian, Gorken Melikian & Charles Cavros	807 s.f.
1-TE-10	Takvor S. Melikian, Gorken Melikian & Charles Cavros	970 s.f.
1-11-T	Stephen Dellaquila & Margaret Dellaquila	500 s.f.
1-TE-11	Stephen Dellaquila & Margaret Dellaquila	850 s.f.
1-12-T	Colby Industrial Supplies Inc.	282 s.f.
1-TE-12	Colby Industrial Supplies Inc.	300 s.f.
1-13-T	Eleanor R. Fortini & Richard A. Fortini	250 s.f.
1-TE-13	Eleanor R. Fortini & Richard A. Fortini	250 s.f.
1-14-T	Joseph E. Gleason	250 s.f.
1-TE-14	Joseph E. Gleason	500 s.f.
1-15-T	Susan F. Whitacre	300 s.f.
1-TE-15	Susan F. Whitacre	600 s.f.
1-16-T	Irving J. Rourke & Eileen M. Rourke	300 s.f.
1-TE-16	Irving J. Rourke & Eileen M. Rourke	600 s.f.

1986	L.O. 6742	In East Longmeadow	Page 17
<u>Parcel</u>	<u>Supposed Owner</u>	<u>Area</u>	
1-17-T	United Petroleum, Inc.	1,459 s.f.	
1-TE-17	United Petroleum, Inc.	3,000 s.f.	
1-18-T	Ernest A. Gralia Jr. & Rosemarie Gralia (Partners) d/b/a E. & R. Realty Co.	310 s.f.	
1-TE-18	Ernest A. Gralia Jr. & Rosemarie Gralia (Partners) d/b/a E. & R. Realty Co.	310 s.f.	
1-19-T	Precision Dental Prosthetics, Inc.	188 s.f.	
1-TE-19	Precision Dental Prosthetics, Inc.	310 s.f.	
1-20-T	Louise M. Siano	62 s.f.	
1-TE-20	Louise M. Siano	250 s.f.	
1-21-T	Evelyn Quieros, Trustee of The Thelma W. Ratner Trust	11 s.f.	
1-TE-21	Evelyn Quieros, Trustee of The Thelma W. Ratner Trust	100 s.f.	
1-22-T	Evelyn Quieros, Trustee of The Thelma W. Ratner Trust	157 s.f.	
1-TE-22	Evelyn Quieros, Trustee of The Thelma W. Ratner Trust	250 s.f.	
1-23-T	Evelyn Quieros, Trustee of The Thelma W. Ratner Trust	3,420 s.f.	
1-TE-23	Evelyn Quieros, Trustee of The Thelma W. Ratner Trust	3,700 s.f.	
1-24-T	Evelyn Quieros, Trustee of The Thelma W. Ratner Trust	460 s.f.	
1-TE-24	Evelyn Quieros, Trustee of The Thelma W. Ratner Trust	1,400 s.f.	
1-25-T	Thomas E. Brunton & Jean M. Brunton	250 s.f.	
1-TE-25	Thomas E. Brunton & Jean M. Brunton	500 s.f.	
1-26-T	William P. Pellegrini & Julia A. Pellegrini	229 s.f.	
1-TE-26	William P. Pellegrini & Julia A. Pellegrini	390 s.f.	

<u>Parcel</u>	<u>Supposed Owner</u>	<u>Area</u>
1-27-T	Eileen F. Daniele	1,070 s.f.
1-TE-27	Eileen F. Daniele	900 s.f.
1-28-T	St. Pauls Evangelical Lutheran Church	197 s.f.
1-TE-28	St. Pauls Evangelical Lutheran Church	575 s.f.
1-29-T	C. Terry Gould & Jane E. Gould H/W	244 s.f.
1-TE-29	C. Terry Gould & Jane E. Gould H/W	300 s.f.
1-30-T	Elmer J. Richards and Alma P. Richards H/W	600 s.f.
1-TE-30	Elmer J. Richards and Alma P. Richards H/W	985 s.f.
1-31-T	Longmeadow Joint Venture Melvin Mandell, Robert Holtzman & Peter J. Berman (Co-partners) d/b/a Cosmopolitan Realty	314 s.f.
1-TE-31	Longmeadow Joint Venture Melvin Mandell, Robert Holtzman & Peter J. Berman (Co-partners) d/b/a Cosmopolitan Realty	800 s.f.
1-32-T	Victor A. Martone, Ralph R. Martone & Elma G. Preston	1,286 s.f.
1-TE-32	Victor A. Martone, Ralph R. Martone & Elma G. Preston	250 s.f.
1-33-T	Gerald E. D'Amour, Paul H. D'Amour and Jeanne E. D'Amour, Trustees of the Big "Y" Trust	298 s.f.
1-TE-33	Tyler Equipment Corporation	9,700 s.f.
1-TE-34	American Saw & Manufacturing Company	780 s.f.

The names of the owners herein given, although supposed to be correct, are such only as matters of opinion and belief.

IT IS THEREFORE VOTED: That said new or existing way, as described herein and as described and shown on said plan, be and the same is hereby laid out by the Massachusetts Department of Public Works in behalf of the Town of East Longmeadow; that the Secretary to the Massachusetts Public Works Commission be and hereby is instructed to file, in the office of the County Commissioners of the County of Hampden and in the office of the Town Clerk

of the Town of East Longmeadow, certified copies of said plan and of this certificate, signed by at least a majority of the members of the Public Works Commission, attesting that said Department

of Public Works has laid out said sections of Town way in accordance with said plan, together with a copy of this adjudication and vote.

Dated at Boston this tenth day of April, 1986.

Members

Alfred S. Gerardo

of the

Ken. D. Doolan

Public

Martha S. Rendon

Works

Commissioner



End of Book ~ State Highways ~ East Longmeadow